

The 4th Division of the Pacific Northwest Region of the  
National Model Railroad Association Presents



**Wednesday, May 11th**

The final **SV&W Model Railroading Clinic** of the modeling season will meet at the Summer Hill location [165 SW 6th Ave] in Oak Harbor Wednesday, May 11th at 7:00pm for a clinic presented by **Ray Wheeler**, former 1:1 railroader and current model railroader.

## "The Lost Art of Railroad Hand Signals"

Ray Wheeler, a former railroader for the Burlington Northern, will instruct us on the use of hand signals in the "real world" of railroading. This will be more than a presentation - you will be an active participant in the evening's clinic!



Ray hired out for the Burlington Northern Operating Department as a brakeman out of Seattle August 26, 1971. This was during the time of transition from hand signals to portable radios when most trains only had one per crew. They are now considered standard issue equipment along with hand lanterns and switch keys. In those days a new hire had the daunting task of not only learning the names and/or numbers of every track in the division, but also a whole list of railroad terminologies, a system of communicating using hand signals, and lots of "Stay Alert and Don't Be Hurt" safety training! It certainly didn't all sink in overnight either!

Ray'll have along some of the signaling equipment (hand lantern, timetable holder, keys, watch, etc...) and a couple of videos to watch as well. You can study the pertinent Consolidated Code of Operating Rules on page 5 beforehand.

Should be fun!

Once again, we will be meeting at **San Remo Mediterranean Grill** in Oak Harbor [421 NE Midway Blvd] for a pre-clinic dinner starting at 5:15 if you are interested. Come and meet our speaker, Ray Wheeler. They have very good pizza, pasta and Spumoni!

## On the calendar:

**Saturday, May 14, 2016: 4D**

**Spring Meet, Tacoma**

Registration on site begins at 8:00 a.m., but you can register online at

[www.wnrr.net/4DPNR/](http://www.wnrr.net/4DPNR/)

[registration.html](http://www.wnrr.net/4DPNR/registration.html)

Clinics start at 9:00 a.m. until 4:00 p.m. The Spring Meet will be held at the Tacoma Community College, 6501 S 19th Street, Tacoma, in Building 15 (Science and Engineering). Parking is available near the building.



**Selkirk Express 2016**

**June 15th - 19th, 2016: Pacific Northwest Region - Selkirk Express 2016 Convention** Salmon Arm, British Columbia, Canada For more information see the official website at: [www.selkirkexpress2016.ca](http://www.selkirkexpress2016.ca)

**July 3 - 10:** The NMRA Convention and National Train Show, Indianapolis, Indiana. See <http://www.nmra2016.org> for more information.



Our "sister" clinic, the Mount Vernon Model Railroad Clinic, meets every second Thursday at the Mount Vernon Senior Center at 1401 Cleveland Street. On May 12th, Kevin Klettke will present "**Fleet Weathering**," a reprise of his clinic at PDX 2015 on efficiently weathering large fleets of rolling stock. Visit Kevin's website at <http://wnrr.net>. Contact Al Carter (360-399-1226 / [tabooma@msn.com](mailto:tabooma@msn.com)) for further information.

"We have a Yahoo Groups page for our use and sharing of information. It is the Skagit Valley and Whidbey Model Railroading group or "sv\_and\_w". The links below will take you directly to the group. In order to post you must have a Yahoo account which is simple to set up.

This is a private group that is not open to the public, it is designed for club use only.

The Link: [http://groups.yahoo.com/group/sv\\_and\\_w](http://groups.yahoo.com/group/sv_and_w) (note those underscores)

If you have general railroading questions or would like to share railroading discussion with immediate locals or are even looking to sell some railroad stuff then this is the place to post information."

## Swap Table

**LAST CHANCE 'TIL SEPTEMBER:** Feel free to bring items to sell, trade or give away to any clinic meeting. Items need to be in the side room by 6:45 and, if for sale or trade, clearly marked as to the price and owner.

## Clinic Schedule for 2015/2016

<del>September 9, 2015</del>	<del>The DuPont Dynamite Narrow Gauge Railway Russ Segner</del>
<del>October 14, 2015</del>	<del>Mini Clinics – various presenters</del>
<del>November 11, 2015</del>	<del>Bob Stafford – Bellingham Trainmaster</del>
<del>December 9, 2015</del>	<del>Planning for the Future of Your Railroad After You've Gone Dick Haines</del>
<del>January 13, 2016</del>	<del>Mystery Make 'n Take &amp; Ice Cream! Susan Gonzales</del>
<del>February 10, 2016</del>	<del>Contest/SHOW and tell – FLAT CAR LOADS Jack Tingstad in charge!</del>
<del>March 9, 2016</del>	<del>Fences are Scenic Structures Too! Al Frasch</del>
<del>April 13, 2016</del>	<del>Tom Hawkins Open House in Anacortes</del>
<b>May 11, 2016</b>	<b>"The Lost Art of Railroad Hand Signals" Ray Wheeler</b>

### ROAD TRIP

Dr. Nick Muff has graciously agreed to open his fabulous Kansas City Southern HO scale model railroad for a tour on Sunday, June 5th at 1pm for our clinic members. Car pooling is encouraged and spouses may attend. Dr. Muff assured that there is plenty of parking. A sign up sheet will be sent around at our May meeting just to get a head count. If you have not seen Dr. Muff's layout before, you are in for a treat. It is incredible. If you have seen it, be prepared for even more additions. We hope all of you can attend this event. The address is: 21310 Rolling Pl., Sedro Woolley, WA This will be a very entertaining day out!

**September 14, 2016**

**We will start up again up in Oak Harbor!**



## Less Than Carload

If your layout wiring looks like this, maybe you need to rethink the way you run your wiring around the layout. . . .



Ever have a train “string-line” around a curve on your layout? Well, it happens on the prototype too:



A empty tanker train derailed at the Port of Tacoma, with 18 cars off the tracks. The train belonged to Tacoma Rail and all of the tank cars were empty.

[From local TV station reporting.]



# Consolidated Code of Operating Rules

Edition Of 1967

## HAND SIGNALS

Note. —When the term "hand signals" is used in these rules it includes flag and lantern signals given by hand.

7. Employees whose duties may require them to give hand signals, must provide themselves with the proper appliances, keep them in good order and ready for immediate use.

7 (A). Employees in train, engine or yard service and others concerned must keep a constant lookout for hand signals. Employees giving hand signals must locate themselves so as to be plainly seen, and give them so as to be plainly understood.

7 (B). Those to whom hand signals are given must act on them promptly and properly. The utmost care must be exercised by employees to avoid acting upon hand signals that may be intended for other trains or engines. Unless employees are positive that hand signals given are for them, they must not move their train or engine until communication is made by words.

7 (C). When backing or pushing a train, engine or cars in response to hand signals, disappearance from view of the employee giving such signals, or the disappearance of the light by which such signals are given, must be regarded as a stop signal, unless employee on leading car has control of air brakes.

When such movements are being controlled by radio communication, failure to maintain radio contact with the employee directing the movement by radio must be regarded as a stop signal.

## HAND, FLAG AND LANTERN SIGNALS

Note.—The hand, or a flag, moved the same as lanterns, gives the same indication.

### Manner of Using

- |  |                      |
|--|----------------------|
| 8. (a) Swung horizontally at right angle to the track. | = stop               |
| 8. (b) Moved slowly with arm extended horizontally.    | = reduce speed       |
| 8. (c) Raised and lowered vertically.                  | = proceed            |
| 8. (d) Swung in a circle at right angle to the track.  | = back               |
| 8. (f) Swung horizontally above the head.              | = apply air brakes   |
| 8. (g) Held at arm's length above the head.            | = release air brakes |

8. (h) Any object waved violently by any person on or near the track must be respected as a signal to stop.

8. (i) Signals prescribed by Rules 8(a) through 8(g) must be used for the purpose described. Other hand signals may be used for other purposes providing they are understood by all members of the crew.

8. (j) Radio, telephone or other means of voice communications may be used instead of hand signals to convey information.

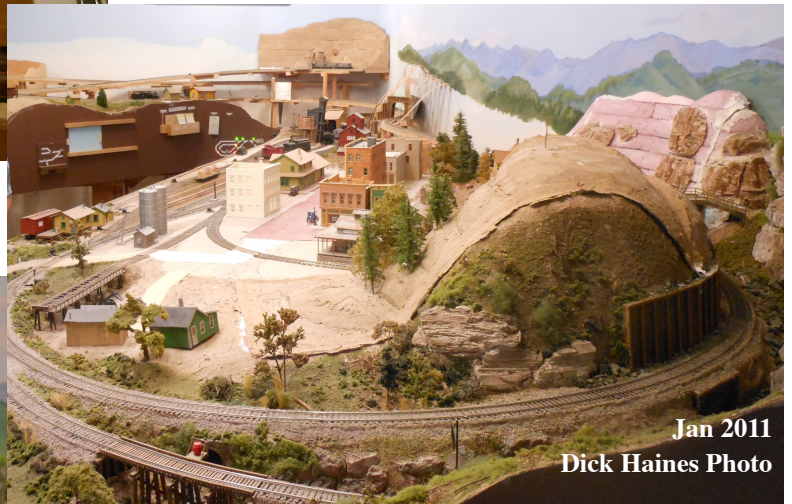


## Local Layout Progress



March 2005  
Dick Haines Photo

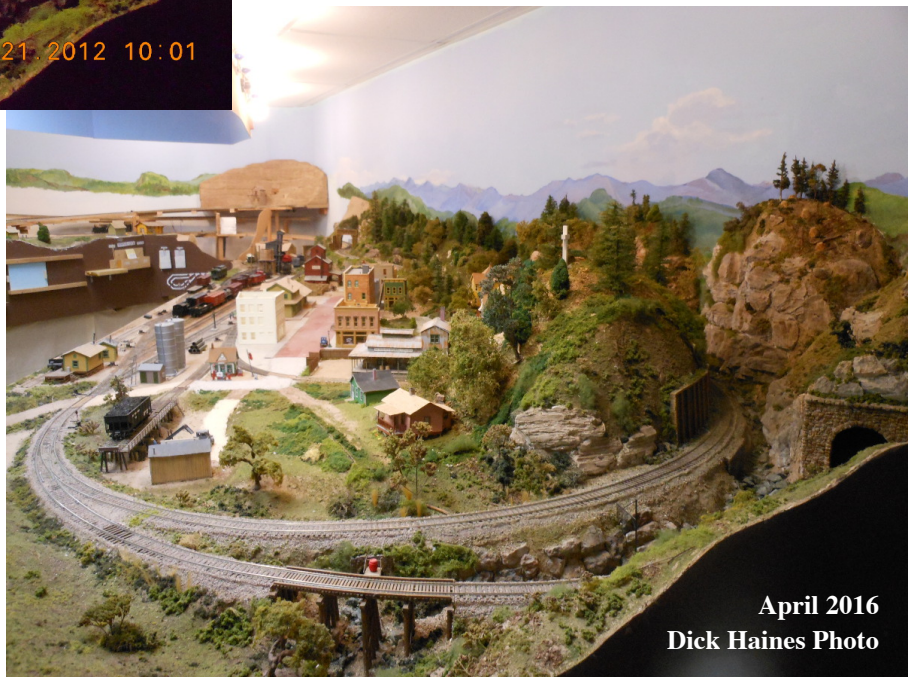
From **Dick Haines**, a pictorial history of part of **Tom Hawkins' layout** over the years of work. We really enjoyed the chance last month to visit his layout - more information is included in Rich Thom's excellent report attached to this newsletter. These photos document the near hillside and town on your right as you enter the layout room.



Jan 2011  
Dick Haines Photo



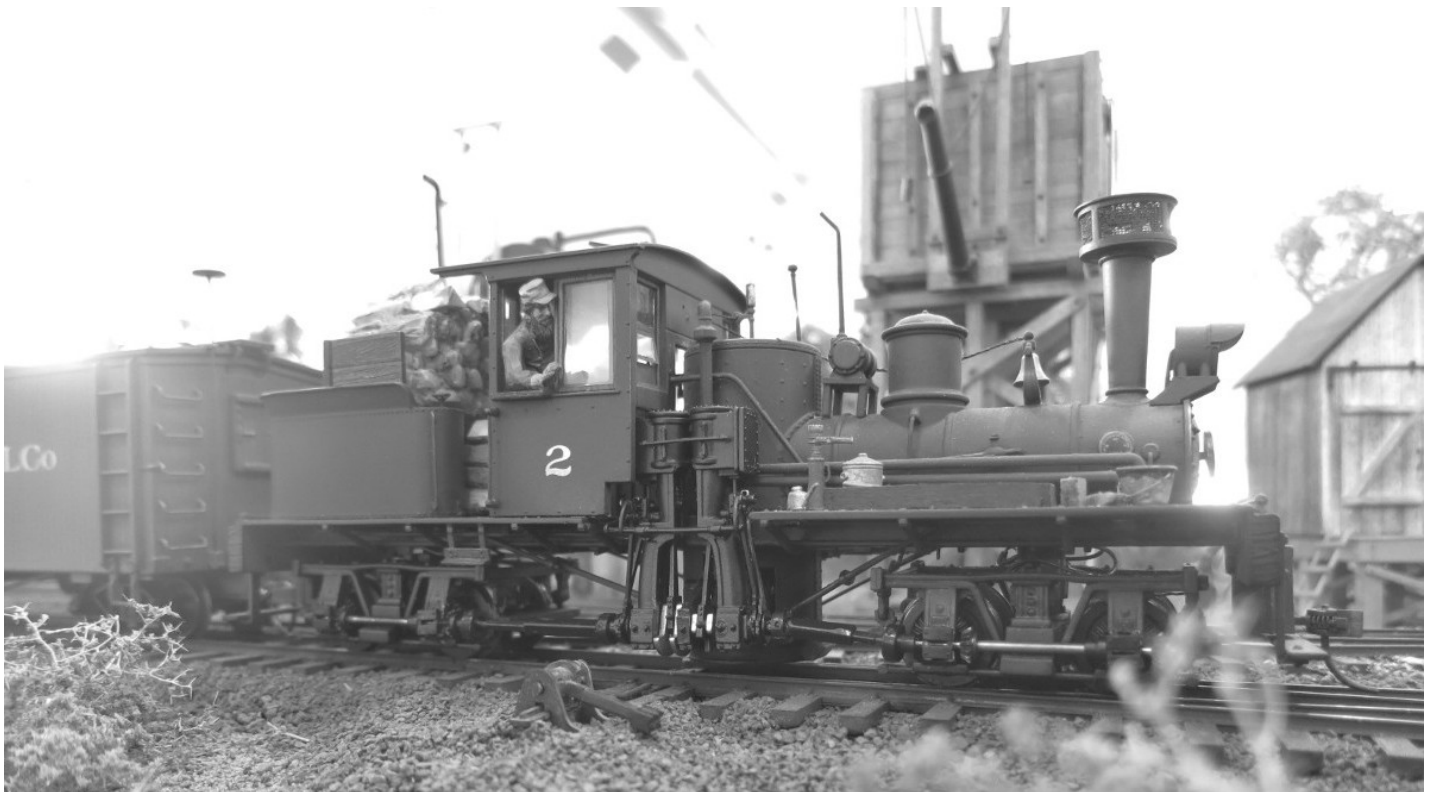
06 21 2012 10:01



April 2016  
Dick Haines Photo



From **Rich Blake**, a look at the fantastic "**Bay Lumber Company**" lokies working at the last Monroe show on the **Pacific Northwest On30 Modular Group** layout. You have to look carefully to note that the B&W pic is a model! Isn't it?!? Thanks, Rich!





And, since this is the last newsletter until September 14th, **something completely different**. I think you will have to look at the track work here a bit to figure out that it is indeed “just” a wye - a one ended wye, but a wye nonetheless. I saw this first in the 3rd Qtr 2015 *NYCentral Modeler* emag, and after doing a bit of searching, found it to be in the city of Carbonia, Sardinia, Italy. Let me know if you decide to model THAT on your layout!





**Skagit Valley and Whidbey Clinic April Meeting**  
**Tom Hawkins' Port Thomas & Southern**  
Article and Photos by Rich Thom

A layout visit makes for a nice change of pace, so on April 13<sup>th</sup> the Skagit Valley and Whidbey NMRA Clinic met at the Anacortes home of long-time member **Tom Hawkins**, for a close-up look at his HO-scale **Port Thomas & Southern**. Since only a few could view the layout at the same time, to keep others busy, and to discourage excessive consumption of the treats supplied every month by **Susan** and **Phil Gonzales** (thanks guys!), clinic members were invited to bring in their track plans—whatever the stage of construction, from dream to done. What a surprise! Of the 23 attendees, there were about as many track plans! Your Grab Iron reporter for one had no idea there were as many layouts abuilding in our area. A lot of interaction and constructive suggestions occurred during the evening.

Tom's layout, though, was the star attraction. The PT&S features a walk-in track plan in a 10 x 20 foot room. Tom's plan won 3<sup>rd</sup> place in Model Railroader's 2012 Track Plan Contest, and was published in the October 2012 issue of MR, pp 64-66.

Tom's railroad was inspired by the prototype Port Townsend & Southern, envisioned by its builders to connect the northern tip of the Olympic Peninsula to Portland, OR. Like many such dreams, it fell far short, only reaching the town of Quilcene 27 miles away.

Tom's plan squeezes a lot of railroad into a modest space. The main line runs from Port Thomas south along the Hood Canal to Shelton, where it interchanges with the Northern Pacific. In addition to Port Thomas, three other towns on the main line are modeled: Mill Town, site of a sawmill; Timber; and Silver Springs. A single-turn helix separates the water-level scenes of Port Thomas and Mill Town from the higher elevations of Timber and Silver Springs. The helix also extends the mainline run.



Fig 1 – Port Thomas

Port Thomas is shown in Fig 1. At the left, the lead to the passenger ship dock; center, freight house; center background, Port Thomas Depot; and at the right, the car float and a cannery. Tom used no. 6 turnouts minimum on the main line and no. 4 minimum for yards and sidings. Many curved turnouts were used, great space-savers. Layout minimum radius is 24". Maximum grade is 2.9% (in the helix) and a challenging 5.3% on the logging line. The backdrop—a stunning feature of the PT&S—was painted by Tom's wife Joyce, a professional artist. Would we all have partners who could create a backdrop like this!



Fig 2 – Passenger and Freight Trains Arrive at Port Thomas Depot

Fig 2 is a tighter view of Port Thomas. Two trains have arrived in town, a passenger train from Shelton and a freight. Tom's layout was designed from the beginning for operations.



Fig 3 – Car Float and Apron

Fig 3 is a detail of the car float, its apron, and tug. (Tom's other lives included a career as an architect and lifelong affection for—and ownership of--boats of all sizes.) The tug is a kit by Model Shipways. A passenger ship is also docked in Port Thomas, out of the photo to the left. Off-layout freight traffic arrives via the car float that connects with the GN, NP and Milwaukee Road in Seattle or Everett. A car float or a ferry is a great scheme for introducing interchange traffic in a small space. Passengers arriving at the other dock also provide good revenue for the PT&S.





Fig 4 – A Small Part of Silver Springs, the Main Town on the PT&S

Silver Springs is the largest town on the PT&S with yard, several businesses to be switched, and engine terminal, only a part of which is shown in Fig 4. Fig 5 shows the busy Silver Springs Depot.



Fig 5 – Silver Springs Depot

A branch line runs from Silver Springs northwest to Port Angeles. Both Shelton and Port Angeles are represented by hidden staging loops below the mountainous scenery behind Timber. At Timber, the PT&S connects with the Timber Ridge Logging & Mining Co., the source of logs for Mill Town; a small silver mine providing ore traffic is also on this line.





Fig 6 – Overall View of Sawmill at Mill Town

Logs from the Timber Ridge show come down the hill to the sawmill complex at Mill Town (Fig 6). Tom drew his own drawings—it helps to be an architect!--for his free-lanced mill. The wigwam sawdust burner is a JV Models kit.



Fig 7 – Log Carriage Detail

Fig 7 shows a detail of the “input end” of the mill, the log hoist and carriage. Most of the mill machinery consists of BTS castings. The logs are from Coastman’s. Tom scratch-built his own dual-circular headsaw using Dremel tool sawblades.

Tom and Joyce moved into their present Anacortes home in 1999, and after a couple of years of planning, Tom began construction of the PT&S in 2002. Tom had a layout in his previous home in Kirkland, and (like a lot of us!) thought he could re-use major sections of his previous layout. None of it, though—or very little—would fit the new space or be useful for his new track plan. One piece of old benchwork was re-used intact, and the previous engine terminal found a new home in the present layout’s town of Silver Springs.





Fig 8 – Operations on the PT&S

From the start, Tom planned operations, and several op sessions have already been held. Crew size is four (including Tom). Fig 8 shows SV&W Clinic members Dick Haines (foreground) and Dave Clarke switching Silver Springs at an op session in 2015. The planned scheme is for two crew members to handle the passenger and freight trains; one the switching duties at Silver Springs and Port Thomas; and the fourth the logging line from a pop-up in the mountain area at one end of the layout. Car cards (from MicroMark) are used along with switch lists for car forwarding. On the logging line, trains run from Timber to Mill Town to the log dump, then to Port Thomas where the engine runs around the empties and returns to Timber and the logging branch.

The SV&W Clinic extends its thanks to Tom for hosting our visit to his fine layout featuring an award-winning track plan, well-executed scenery with an exceptional backdrop, craftsman-kit and scratch-built structures, and operations as well.

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